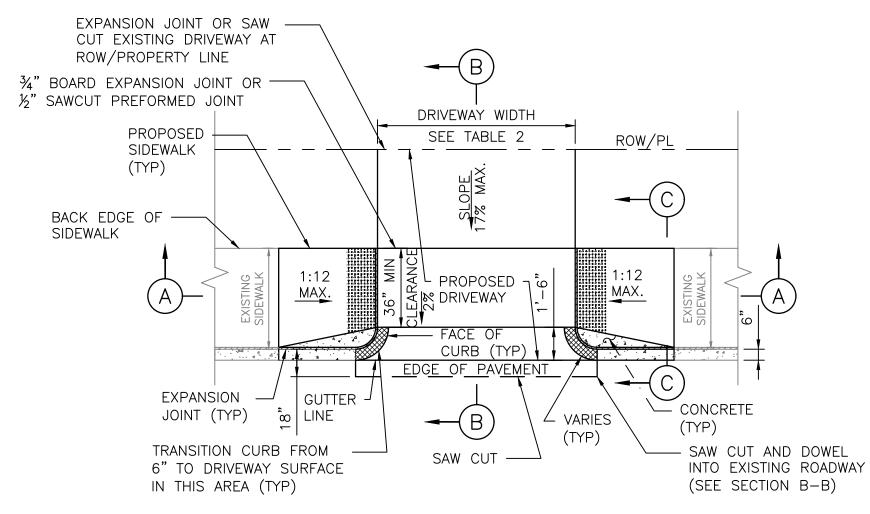
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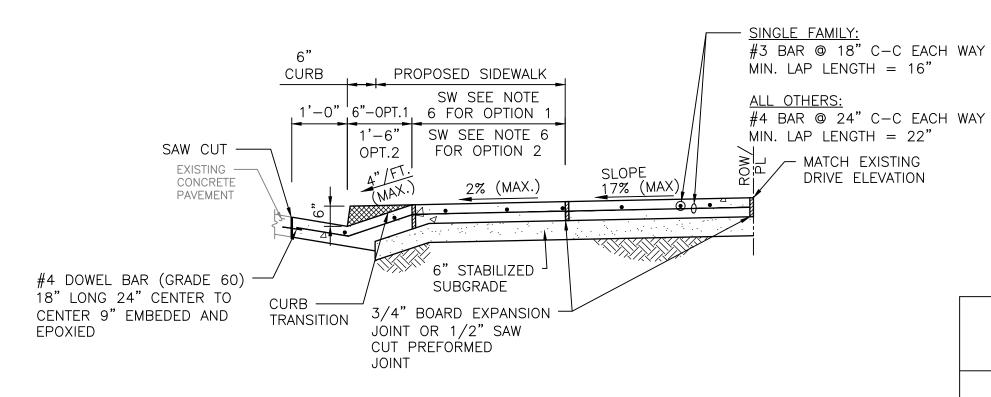
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ENGINEERING PRACTICE REVIEW THESE DESIGN NN FOR ANY PURPOSES

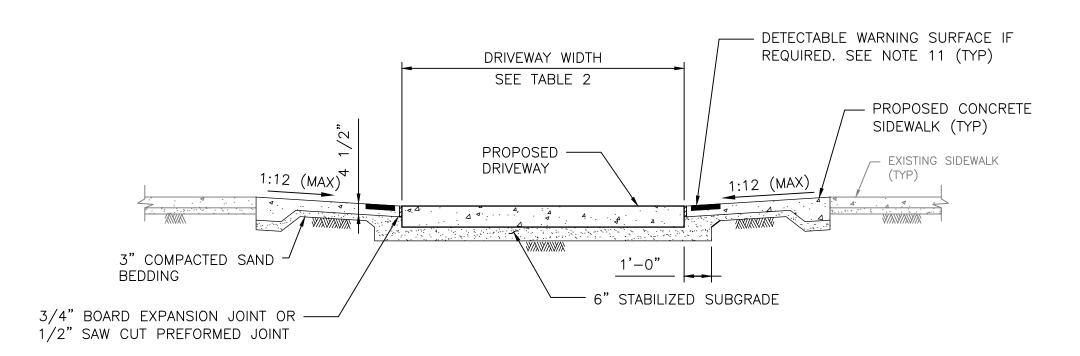
OPTION 1 — CURB LEVEL SIDEWALK PLAN VIEW - DRIVEWAY WITHOUT CURB AT CURB RETURN



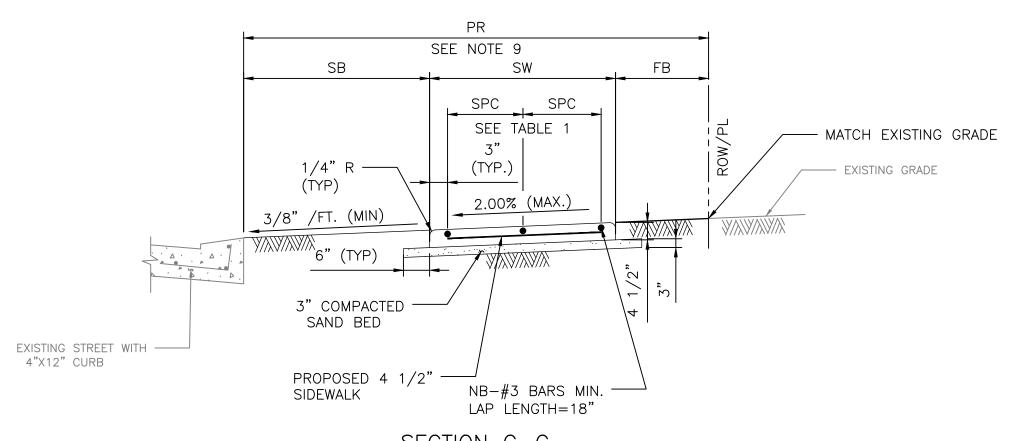
<u>OPTION 2 — CURB LEVEL SIDEWALK</u> <u>PLAN VIEW - DRIVEWAY WITH CURB AT CURB RETURN</u>



SECTION B-B TYPICAL DRIVEWAY SECTION THROUGH DRIVEWAY



SECTION A-A PROPOSED SIDEWALK THROUGH DRIVEWAY



SECTION C-C TYPICAL SIDEWALK SECTION

TABLE 1

REINFORCING STEEL INFORMATION FOR 4 ½ " THICK SIDEWALKS EXPANSION JOINT SPACING = 40 FT fc' = 3,500 PSI AND fy = 60,000 PSI

REFER TO CONTRACT DRAWINGS FOR SIDEWALKS WIDER THAN 6 FEET.

SIDEWALK THICKNESS (IN)		L	ONGITUDINAL S			
	SIDEWALK		#3 BARS	TRANSVERSE STEE		
	S WIDTH (FT)	NO. OF BARS "NB"	SPACING "SPC" (IN)	END BAR SPACING (IN)	#3 BARS SPACING (IN)	
4.5	5	3	27	3	48	
4.5	6	4	22	3	48	
	·		·	·	·	

TABLE 2

				DRIVEW	/AY DES	IGN CRI	TERIA ⁽¹⁾)(2)				
RAFFIC TYPE.	(FOR SINGLE	TYPE A DR FAMILY RES DUPLEX	SIDENTIAL HO	OUSES OR	TYPE B DRIVEWAY (SHARED ACCESS/SHARED DRIVEWAY)			VEWAY)	TYPE C DRIVEWAY (COMMERCIAL DRIVEWAY)			
(,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,	WIDTH	(FT)	RADIU:	S (FT)	WIDTH	I (FT)	RADIU	S (FT)	WIDTH	(FT)	RADIUS	S (FT)
	MIN	MAX	MIN	MAX	MIN	MAX	MIN	MAX	MIN	MAX	MIN	MAX
ONE-WAY	10	12	4	10	12 ⁽⁵⁾	16 ⁽⁵⁾	4 ⁽⁵⁾	10 ⁽⁵⁾	15	20	10	20
TWO-WAY	10 ⁽³⁾	24 ⁽⁴⁾	4	10	16 ⁽⁶⁾	24	4	10	24	35	10	20

- (1) REFER TO INFRASTRUCTURE DESIGN MANUAL ARTICLE 15.2.07.C.1.F FOR DRIVEWAYS THAT REQUIRE A VEHICLE SWEPT PATH ANALYSIS.
- (2) REFER TO INFRASTRUCTURE DESIGN MANUAL ARTCILES 15.2.07.C.1.G.(1) AND 15.2.07.C.1.G.(2) FOR TYPE 1 PAE AND TYPE 2 PAE REQUIREMENTS.
- (3) THE MINIMUM WIDTH FOR JOINT ACCESS DRIVEWAY IS 12 FT.
- (4) REFER TO CHAPTER 42 OF THE CODE OF ORDINANCES FOR DRIVEWAY WIDTHS FOR NARROW LOTS.
- (5) ONLY MURS AND COURTYARD STYLE DEVELOPMENTS ON CORNER LOTS CAN HAVE ONE-WAY DRIVEWAYS.
- (6) REFER TO CHAPTER 42, SECTION 42-146 OF THE CODE OF ORDINANCES FOR EXCEPTIONS TO THE MINIMUM DRIVEWAY WIDTH FOR SHARED DRIVEWAYS.

NOTES:

OF CODE OF ORDINANCES SECTION 40-552.

OF EXISTING SIDEWALKS:

- 2.1. EXISTING SIDEWALKS LESS THAN OR EQUAL TO 20 FEET IN TOTAL LENGTH:
 - 2.1.1. THE PROPOSED SIDEWALK WIDTH WILL BE ALLOWED TO MATCH THE EXISTING SIDEWALK.
- 2.2. EXISTING SIDEWALKS GREATER THAN 20 FEET IN TOTAL LENGTH:
 - 2.2.1. THE SIDEWALK WIDTH FOR THE ENTIRE PROPERTY WIDTH SHALL BE IMPROVED TO MEET WIDTH REQUIREMENTS ACCORDING TO THE LATEST INFRASTRUCTURE DESIGN MANUAL.
- 2.3. 20 FOOT TOTAL LENGTH IS DEFINED AS:
 - 2.3.1. UP TO 10 FEET ON BOTH SIDES OF THE DRIVEWAY; OR
 - 2.3.2. UP TO 20 FEET WHEN SIDEWALK AFFECTED IS LOCATED ONLY ON ONE SIDE OF THE DRIVEWAY.
- 3. PROPOSED SIDEWALKS THAT DO NOT FALL WITHIN THE SCOPE DEFINED IN NOTE 2 SHALL BE DESIGNED ACCORDING TO IDM REQUIREMENTS AND CONSTRUCTED ACCORDING TO THE CONTRACT DRAWINGS.
- 4. DRIVEWAY WIDTH VARIES TO MATCH EXISTING
- 5. DRIVEWAY DETAIL TO PROVIDE FOR SIDEWALK TRANSITION FROM EXISTING BACK OF CURB TO PROPOSED DRIVEWAY EDGE.
- 6. DRIVEWAY WITH WIDTH >5' @ 2% CROSS SLOPE QUALIFIES AS PASSING SPACE. (SEE OPTION 1 AND OPTION 2 PLANS)
- 7. ALL RAMPS AND SIDEWALKS/WALKWAYS SHALL BE CONSTRUCTED IN ACCORDANCE WITH HOUSTON PUBLIC WORKS STANDARDS, TEXAS ACCESSIBILITY STANDARDS (TAS) AND AMERICANS WITH DISABILITIES ACT (ADA) REQUIREMENTS. IF THERE IS A CONFLICT IN THE REQUIREMENTS, THE STRICTEST REQUIREMENTS SHALL GOVERN.

- 1. REPAIR, RECONSTRUCTION OR REPLACEMENT OF 8. CURB RAMPS THAT ARE STEEPER THAN A 1:12 SIDEWALKS SHALL MEET PERMITTING REQUIREMENTS MAX SLOPE WILL NOT BE ACCEPTED BY THE CITY OF HOUSTON.
- 2. FOR REPAIR, RECONSTRUCTION, OR REPLACEMENT 9. REFER TO CONTRACT DRAWINGS FOR PEDESTRIAN REALM (PR), SIDEWALK (SW), FRONTAGE BUFFER (FB), AND SAFETY BUFFER (SB) WIDTHS.
 - 10. DRIVEWAYS SHALL BE MINIMUM 6" THICK FOR SINGLE FAMILY AND DUPLEXES. DRIVEWAYS SHALL BE MINIMUM 7" THICK FOR ALL OTHERS (I.E. COMMERCIAL, INDUSTRIAL, ETC.)
 - 11. DETECTABLE WARNING SURFACES:
 - 11.1. SIDEWALK SHALL HAVE A DETECTABLE WARNING SURFACE WHERE:
 - 11.1.1. SIDEWALK INTERSECTS TYPE C DRIVEWAYS (COMMERCIAL DRIVEWAYS) THAT ARE STOP, YIELD OR TRAFFIC SIGNAL CONTROLLED;
 - 11.1.2. SIDEWALK SLOPE IS GREATER THAN 1:20 AND INTERSECTS A TYPE C DRIVEWAY (COMMERCIAL DRIVEWAY)
 - 11.2. DETECTABLE WARNING SURFACES ARE OPTIONAL WHERE SIDEWALKS INTERSECT TYPE A DRIVEWAYS (SINGLE FAMILY RESIDENTIAL HOUSES OR DUPLEXES) OR TYPE B DRIVEWAYS (SHARED ACCESS/SHARED DRIVEWAYS).
 - 11.3. REFER TO STANDARD DETAILS 02775-06 TO 02775-07 FOR DETECTABLE WARNING SURFACE STANDARDS.

APPROVED BY:	APPROVED BY:
Docusigned by: Sulvail Lanwar 9EF8B0C641F5478	DocuSigned by: LHING NGUYEN 95A29EFDA75B4CD
CITY ENGINEER	CITY TRAFFIC ENGINEER
APPROVED BY: DocuSigned by: Larof Haddock A93C410B72B3453	
DIRECTOR OF HOL	JSTON PUBLIC WORKS
EFF DATE: NOV-27-2023	DWG NO: 02754-03

PROPOSED SIDEWALK THROUGH DRIVEWAY WITH EXCESSIVE FLEVATION DIFFERENCE

	DITTERCENCE
	FOR CITY OF HOUSTON USE ON
	1
DRAWING SCALE]
NOT TO SCALE	