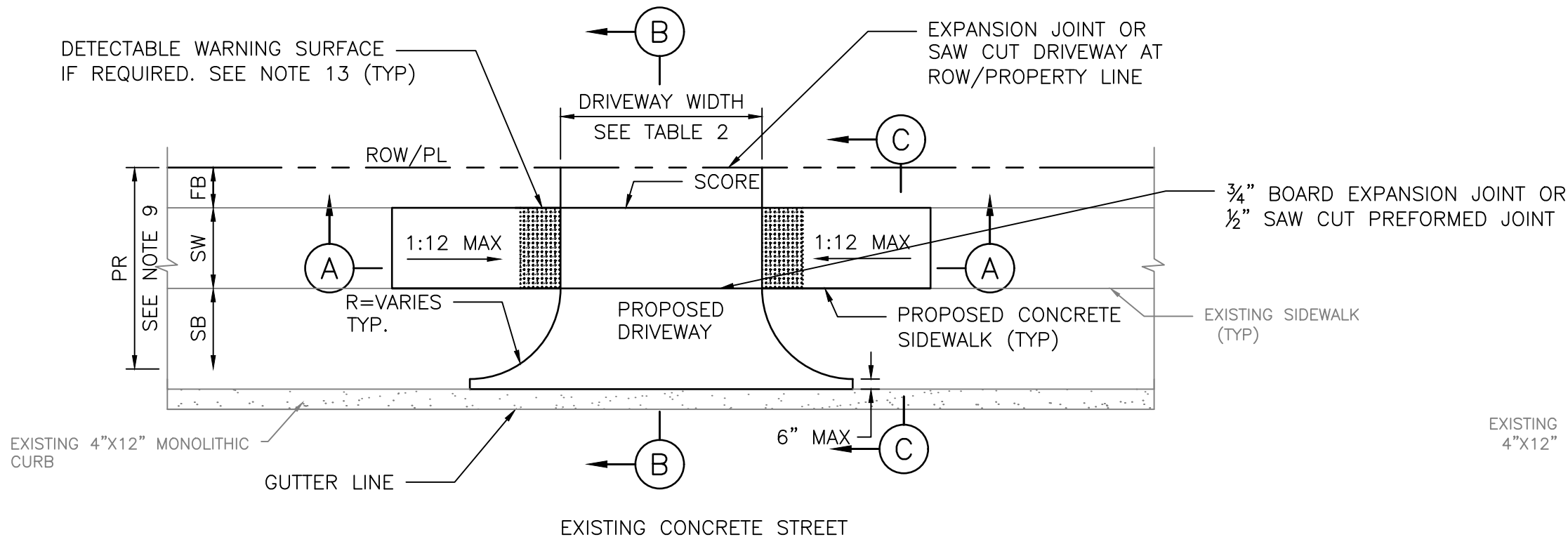
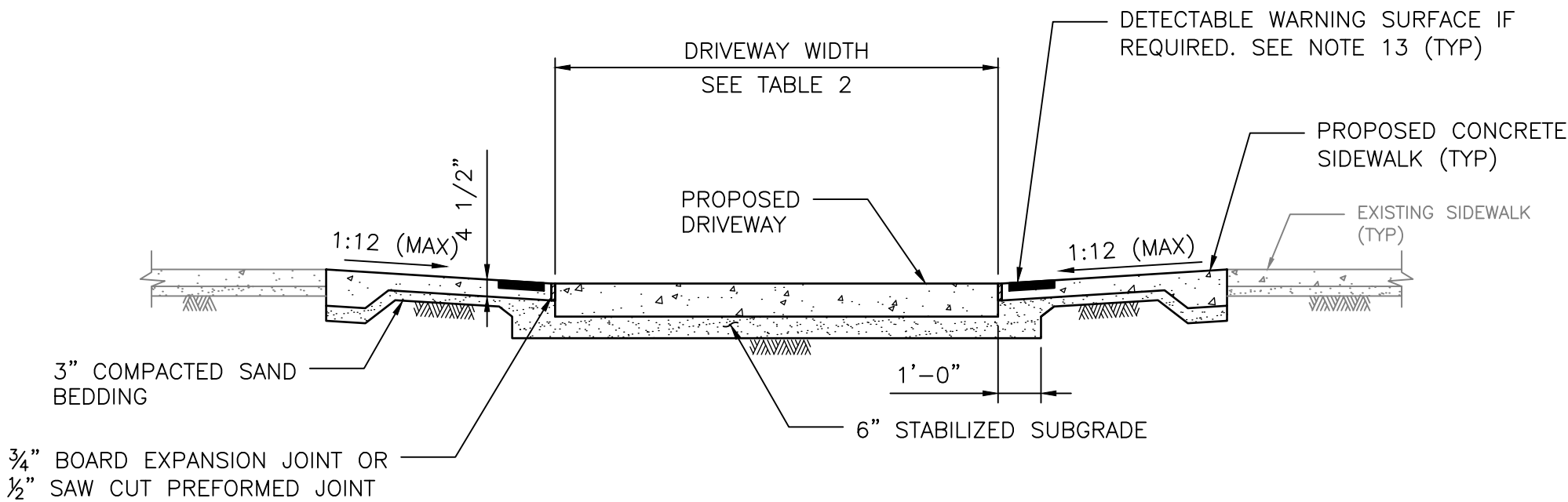


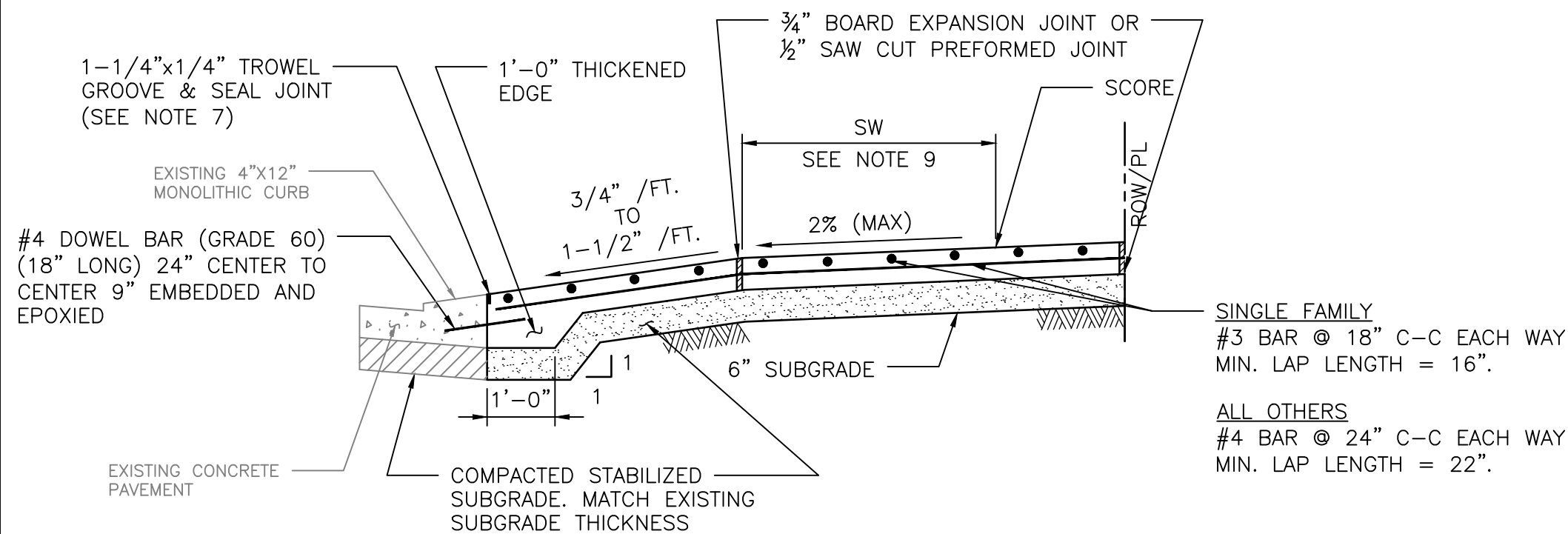
DISCLAIMER: THIS STANDARD IS GOVERNED BY THE TEXAS ENGINEERING PRACTICE ACT. THE DESIGN REQUIREMENTS ON THIS STANDARD DO NOT PURPORT TO ADDRESS ALL OF THE SAFETY CONCERNS ASSOCIATED WITH THE USE OF THIS STANDARD. THE ENGINEER OF RECORD (EOR) IS TO REVIEW THESE DESIGN REQUIREMENTS AND BY AUTHORIZING THEIR USE, ACCEPTS RESPONSIBILITY FOR THEIR APPLICABILITY, ADEQUACY AND SAFETY. NO WARRANTY OF ANY KIND IS MADE BY THE CITY OF HOUSTON FOR ANY PURPOSES WHATSOEVER. THE CITY OF HOUSTON ASSUMES NO RESPONSIBILITY FOR INCORRECT RESULTS OR DAMAGES RESULTING FROM ITS USE.



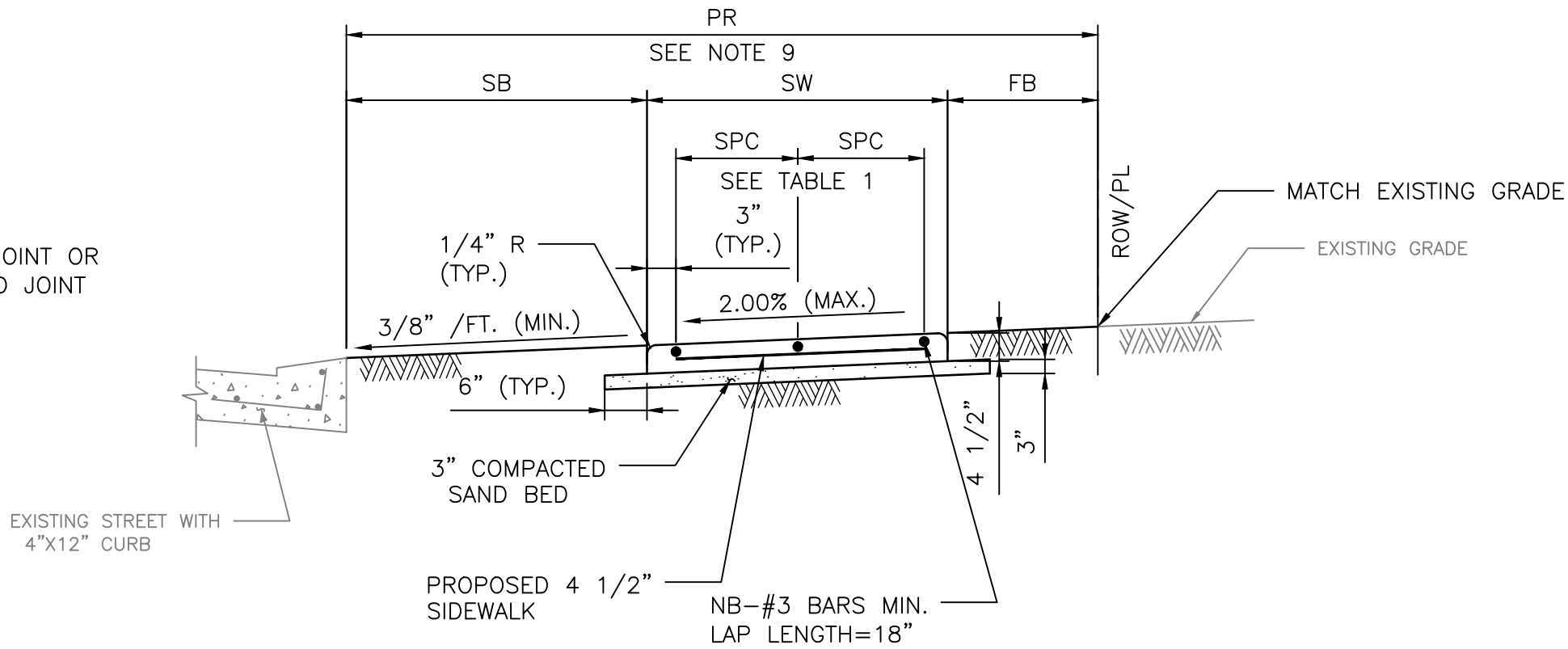
PLAN VIEW
DRIVEWAY



SECTION A-A
PROPOSED SIDEWALK, THROUGH DRIVEWAY WITH
EXCESSIVE GRADES



SECTION B-B
TYPICAL DRIVEWAY SECTION



SECTION C-C
TYPICAL SIDEWALK SECTION

TABLE 1

REINFORCING STEEL INFORMATION
FOR 4 1/2" THICK SIDEWALKS
EXPANSION JOINT SPACING = 40 FT
fc' = 3,500 PSI AND fy = 60,000 PSI
REFER TO CONTRACT DRAWINGS FOR SIDEWALKS WIDER THAN 6 FEET.

SIDEWALK THICKNESS (IN)	SIDEWALK WIDTH (FT)	LONGITUDINAL STEEL			TRANSVERSE STEEL #3 BARS SPACING (IN)
		NO. OF BARS "NB"	SPACING "SPC" (IN)	END BAR SPACING (IN)	
4.5	5	3	27	3	48
4.5	6	4	22	3	48

TABLE 2

DRIVEWAY DESIGN CRITERIA (1)(2)												
TRAFFIC TYPE	TYPE A DRIVEWAY (FOR SINGLE FAMILY RESIDENTIAL HOUSES OR DUPLEXES)				TYPE B DRIVEWAY (SHARED ACCESS/SHARED DRIVEWAY)				TYPE C DRIVEWAY (COMMERCIAL DRIVEWAY)			
	WIDTH (FT)		RADIUS (FT)		WIDTH (FT)		RADIUS (FT)		WIDTH (FT)		RADIUS (FT)	
	MIN	MAX	MIN	MAX	MIN	MAX	MIN	MAX	MIN	MAX	MIN	MAX
ONE-WAY	10	12	4	10	12 ⁽⁵⁾	16 ⁽⁵⁾	4 ⁽⁵⁾	10 ⁽⁵⁾	15	20	10	20
TWO-WAY	10 ⁽³⁾	24 ⁽⁴⁾	4	10	16 ⁽⁶⁾	24	4	10	24	35	10	20

(1) REFER TO INFRASTRUCTURE DESIGN MANUAL ARTICLE 15.2.07.C.1.F FOR DRIVEWAYS THAT REQUIRE A VEHICLE SWEEP PATH ANALYSIS.

(2) REFER TO INFRASTRUCTURE DESIGN MANUAL ARTICLES 15.2.07.C.1.G.(1) AND 15.2.07.C.1.G.(2) FOR TYPE 1 PAE AND TYPE 2 PAE REQUIREMENTS.

(3) THE MINIMUM WIDTH FOR JOINT ACCESS DRIVEWAY IS 12 FT.

(4) REFER TO CHAPTER 42 OF THE CODE OF ORDINANCES FOR DRIVEWAY WIDTHS FOR NARROW LOTS.

(5) ONLY MURS AND COURTYARD STYLE DEVELOPMENTS ON CORNER LOTS CAN HAVE ONE-WAY DRIVEWAYS.

(6) REFER TO CHAPTER 42, SECTION 42-146 OF THE CODE OF ORDINANCES FOR EXCEPTIONS TO THE MINIMUM DRIVEWAY WIDTH FOR SHARED DRIVEWAYS.

NOTES:

- REPAIR, RECONSTRUCTION OR REPLACEMENT OF SIDEWALKS SHALL MEET PERMITTING REQUIREMENTS OF CODE OF ORDINANCES SECTION 40-552.
- FOR REPAIR, RECONSTRUCTION, OR REPLACEMENT OF EXISTING SIDEWALKS:
 - EXISTING SIDEWALKS LESS THAN OR EQUAL TO 20 FEET IN TOTAL LENGTH:
 - THE PROPOSED SIDEWALK WIDTH WILL BE ALLOWED TO MATCH THE EXISTING SIDEWALK.
 - EXISTING SIDEWALKS GREATER THAN 20 FEET IN TOTAL LENGTH:
 - THE SIDEWALK WIDTH FOR THE ENTIRE PROPERTY WIDTH SHALL BE IMPROVED TO MEET WIDTH REQUIREMENTS ACCORDING TO THE LATEST INFRASTRUCTURE DESIGN MANUAL.
 - 20 FOOT TOTAL LENGTH IS DEFINED AS:
 - UP TO 10 FEET ON BOTH SIDES OF THE DRIVEWAY; OR
 - UP TO 20 FEET WHEN SIDEWALK AFFECTED IS LOCATED ONLY ON ONE SIDE OF THE DRIVEWAY.
- IF AVAILABLE ROW IS NOT SUFFICIENT TO ACCOMMODATE SIDEWALK WIDTH (SW) ACCORDING TO IDM REQUIREMENTS, ENGINEER SHALL OBTAIN A VARIANCE FROM THE CITY ENGINEER.
- DRIVEWAYS SHALL BE 6" THICK FOR SINGLE FAMILY OR DUPLEXES.
- DRIVEWAYS AND SIDEWALKS SHALL BE CONSTRUCTED WITH PORTLAND CEMENT CONCRETE AND INCLUDE 5 1/2 SACKS OF CEMENT PER CUBIC YARD OF CONCRETE.
- THE OUTER DOWEL BARS ARE TO BE LOCATED 12" FROM END OF PROPOSED EDGE OF DRIVEWAY RETURN. EXTEND DOWEL 3" INCHES INTO PROPOSED DRIVEWAY AND BEND REMAINING BAR TO EXTEND TO RADIUS RETURN BOTH SIDES.
- TROWEL GROOVE SEALANT SHALL BE LOW MODULUS SILICONE OR POLYURETHANE SEALANT.
- ALL JOINTS ALONG THE SIDEWALK SHALL BE CONSTRUCTED ACCORDING TO DRAWING 02752-02 AND SPECIFICATION 02752.
- REFER TO CONTRACT DRAWINGS FOR PEDESTRIAN REALM (PR), SIDEWALK (SW), FRONTAGE BUFFER (FB), AND SAFETY BUFFER (SB) WIDTHS.
- CEMENT STABILIZED SAND 1.5 SACKS OF CEMENT PER TON OF DRY SAND.
- ALL RAMPS AND SIDEWALKS/WALKWAYS SHALL BE CONSTRUCTED IN ACCORDANCE WITH HOUSTON PUBLIC WORKS STANDARDS, TEXAS ACCESSIBILITY STANDARDS (TAS) AND AMERICANS WITH DISABILITIES ACT (ADA) REQUIREMENTS. IF THERE IS A CONFLICT IN THE REQUIREMENTS, THE STRICTEST REQUIREMENTS SHALL GOVERN.
- CURB RAMPS THAT ARE STEEPER THAN A 1:12 MAX SLOPE WILL NOT BE ACCEPTED BY THE CITY OF HOUSTON.
- DETECTABLE WARNING SURFACES:
 - SIDEWALK SHALL HAVE A DETECTABLE WARNING SURFACE WHERE:
 - SIDEWALK INTERSECTS TYPE C DRIVEWAYS (COMMERCIAL DRIVEWAYS) THAT ARE STOP, YIELD OR TRAFFIC SIGNAL CONTROLLED; OR
 - SIDEWALK SLOPE IS GREATER THAN 1:20 AND INTERSECTS A TYPE C DRIVEWAY (COMMERCIAL DRIVEWAY)
 - DETECTABLE WARNING SURFACES ARE OPTIONAL WHERE SIDEWALKS INTERSECT TYPE A DRIVEWAYS (SINGLE FAMILY RESIDENTIAL HOUSES OR DUPLEXES) OR TYPE B DRIVEWAYS (SHARED ACCESS/SHARED DRIVEWAYS).
- REFER TO STANDARD DETAILS 02775-06 TO 02775-07 FOR DETECTABLE WARNING SURFACE STANDARDS.

APPROVED BY:	APPROVED BY:
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CITY ENGINEER	CITY TRAFFIC ENGINEER

APPROVED BY:
<div>DocuSigned by: Carl Hallack A98C410B72B3453</div>
DIRECTOR OF HOUSTON PUBLIC WORKS

EFF DATE: NOV-27-2023	DWG NO: 02754-01A
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CITY OF HOUSTON
HOUSTON PUBLIC WORKS STANDARD

DRIVEWAY DETAIL WITH
4"x12" CURB FOR LOCAL
RESIDENTIAL STREETS

	FOR CITY OF HOUSTON USE ONLY
DRAWING SCALE	
NOT TO SCALE	