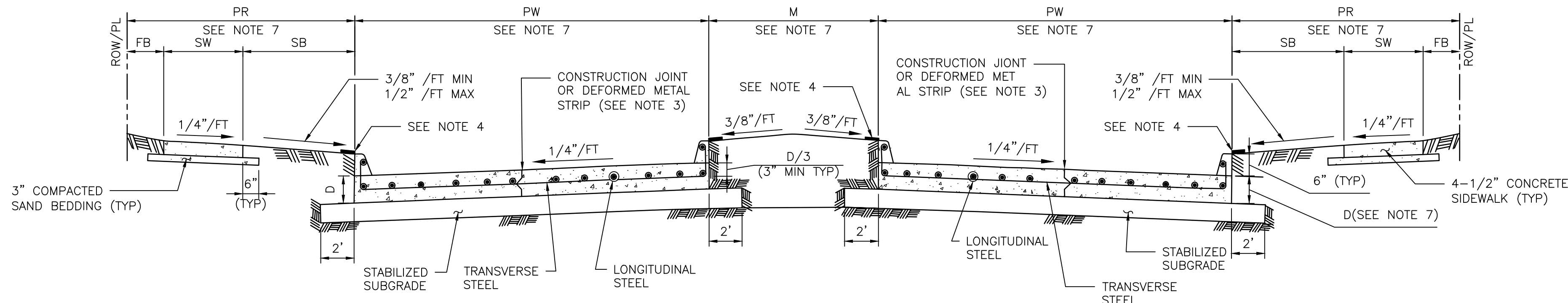
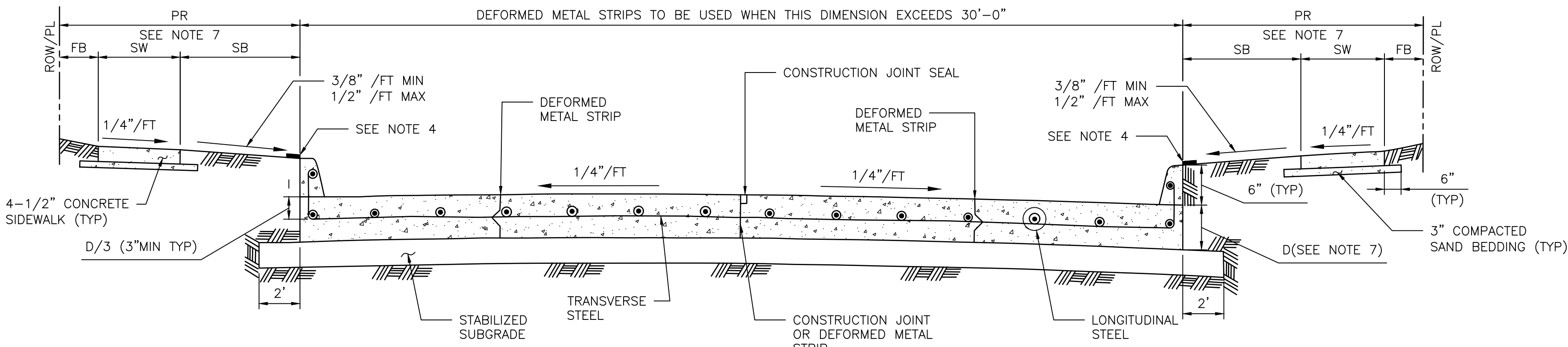


DISCLAIMER: THIS STANDARD IS GOVERNED BY THE TEXAS ENGINEERING PRACTICE ACT. THE DESIGN REQUIREMENTS ON THIS STANDARD DO NOT PURPORT TO ADDRESS ALL OF THE SAFETY CONCERNS ASSOCIATED WITH THE USE OF THIS STANDARD. THE ENGINEER OF RECORD (EOR) IS TO REVIEW THESE DESIGN REQUIREMENTS AND BY AUTHORIZING THEIR USE, ACCEPTS RESPONSIBILITY FOR THEIR APPLICABILITY, ADEQUACY AND SAFETY. NO WARRANTY OF ANY KIND IS MADE BY THE CITY OF HOUSTON FOR ANY PURPOSES WHATSOEVER. THE CITY OF HOUSTON ASSUMES NO RESPONSIBILITY FOR INCORRECT RESULTS OR DAMAGES RESULTING FROM ITS USE.



TYPICAL DOUBLE ROADWAY SECTION FOR CONCRETE PAVEMENT WITH CURBS



TYPICAL SINGLE ROADWAY SECTION FOR CONCRETE PAVEMENT WITH CURBS

TABLE 1
REINFORCING STEEL BAR SIZES AND SPACINGS FOR VARIOUS PAVEMENT THICKNESSES (D) WITH: MAXIMUM TRANSVERSE CONTROL JOINT SPACING = 20'-0"
MAXIMUM EXPANSION JOINT SPACING = 80'-0"
f_c = 4,000 PSI/28 DAYS AND F_y = 60,000 PSI

| PAVEMENT THICKNESS 'D' (IN) | PAVEMENT WIDTH 'PW' (FT) | LONGITUDINAL STEEL | | | | | | | | | TRAVERSE STEEL | | |
|-----------------------------|--------------------------|--------------------|--------------|----------------------|----------------|--------------|----------------------|----------------|--------------|----------------------|----------------|--------------|--------------|
| | | #4 BARS | | | #5 BARS | | | #6 BARS | | | #4 BARS | #5 BARS | #6 BARS |
| | | NUMBER OF BARS | SPACING (IN) | END BAR SPACING (IN) | NUMBER OF BARS | SPACING (IN) | END BAR SPACING (IN) | NUMBER OF BARS | SPACING (IN) | END BAR SPACING (IN) | SPACING (IN) | SPACING (IN) | SPACING (IN) |
| 6 | 28 | 17 | 20.50 | 4.00 | - | - | - | - | - | - | 36 | - | - |
| 7 | 25 | 17 | 18.25 | 4.00 | - | - | - | - | - | - | 36 | - | - |
| 7 | 35 | 24 | 18.00 | 3.00 | - | - | - | - | - | - | 36 | - | - |
| 7 | 36 | 25 | 17.75 | 3.00 | - | - | - | - | - | - | 36 | - | - |
| 7 | 37 | 25 | 18.25 | 3.00 | - | - | - | - | - | - | 36 | - | - |
| 7 | 41 | 28 | 18.00 | 3.00 | - | - | - | - | - | - | 36 | - | - |
| 7 | 45 | 31 | 17.75 | 3.75 | - | - | - | - | - | - | 36 | - | - |
| 8 | 25 | 20 | 15.50 | 2.75 | 13 | 24.50 | 3.0 | - | - | - | 36 | 36 | - |
| 8 | 34 | 27 | 15.50 | 2.50 | 17 | 25.00 | 4.0 | - | - | - | 36 | 36 | - |
| 8 | 35 | 27 | 16.00 | 2.00 | 18 | 24.25 | 4.0 | - | - | - | 36 | 36 | - |
| 8 | 36 | 28 | 15.75 | 3.25 | 18 | 25.00 | 3.0 | - | - | - | 30 | 36 | - |
| 8 | 44 | 24 | 15.75 | 4.00 | 22 | 24.75 | 4.0 | - | - | - | 30 | 36 | - |
| 8 | 45 | 35 | 15.75 | 2.25 | 23 | 24.25 | 3.0 | - | - | - | 30 | 36 | - |
| 9 | 25 | 22 | 14.00 | 3.00 | 14 | 22.50 | 4.0 | - | - | - | 36 | 36 | - |
| 9 | 34 | 31 | 13.50 | 2.00 | 19 | 22.25 | 3.5 | - | - | - | 30 | 36 | - |
| 9 | 35 | 31 | 13.75 | 3.75 | 20 | 21.75 | 3.5 | - | - | - | 30 | 36 | - |
| 9 | 36 | 32 | 13.75 | 3.00 | 21 | 21.25 | 3.5 | - | - | - | 30 | 36 | - |
| 9 | 44 | 39 | 13.75 | 2.75 | 25 | 21.75 | 3.0 | - | - | - | 24 | 36 | - |
| 9 | 45 | 39 | 14.00 | 4.00 | 26 | 21.25 | 4.5 | - | - | - | 24 | 36 | - |
| 10 | 25 | 24 | 12.75 | 3.50 | 17 | 18.25 | 4.0 | - | - | - | 36 | 36 | 36 |
| 10 | 34 | 33 | 12.50 | 4.00 | 21 | 20.00 | 4.0 | - | - | - | 30 | 36 | 36 |
| 10 | 35 | 34 | 12.50 | 3.75 | 23 | 18.75 | 4.0 | - | - | - | 30 | 36 | 36 |
| 10 | 36 | 35 | 12.50 | 3.50 | 24 | 18.50 | 3.0 | - | - | - | 30 | 36 | 36 |
| 10 | 44 | 44 | 12.00 | 4.00 | 29 | 18.50 | 4.5 | - | - | - | 24 | 36 | 36 |
| 10 | 45 | 44 | 12.50 | 3.00 | 29 | 19.00 | 3.0 | - | - | - | 24 | 36 | 36 |
| 11 | 25 | 27 | 11.25 | 3.00 | 17 | 18.25 | 4.0 | 12 | 26.75 | 3 | 36 | 36 | 36 |
| 11 | 34 | 36 | 11.50 | 2.75 | 24 | 17.50 | 2.5 | 17 | 25.00 | 4 | 24 | 36 | 36 |
| 11 | 35 | 37 | 11.50 | 3.00 | 24 | 18.00 | 3.0 | 17 | 25.75 | 4 | 24 | 36 | 36 |
| 11 | 36 | 40 | 11.00 | 2.00 | 25 | 17.75 | 3.0 | 17 | 26.50 | 4 | 24 | 36 | 36 |
| 11 | 44 | 48 | 11.125 | 2.50 | 30 | 18.00 | 3.0 | 21 | 26.00 | 4 | 24 | 36 | 36 |
| 11 | 45 | 49 | 11.125 | 3.00 | 31 | 17.75 | 4.0 | 22 | 25.50 | 3 | 24 | 36 | 36 |
| 12 | 25 | - | - | - | 19 | 16.25 | 4.0 | 13 | 24.50 | 3 | 36 | 36 | 36 |
| 12 | 34 | - | - | - | 26 | 16.00 | 4.0 | 18 | 23.50 | 4 | 24 | 36 | 36 |
| 12 | 35 | - | - | - | 26 | 16.50 | 4.0 | 19 | 23.00 | 3 | 24 | 36 | 36 |
| 12 | 36 | - | - | - | 27 | 16.25 | 4.5 | 20 | 22.25 | 4.5 | 24 | 36 | 36 |
| 12 | 44 | - | - | - | 33 | 16.25 | 4.0 | 24 | 22.50 | 5 | 24 | 30 | 36 |
| 12 | 45 | - | - | - | 35 | 15.75 | 3.0 | 25 | 22.25 | 3 | 24 | 30 | 36 |

MINIMUM LAP LENGTH (L):
A. # 4 BARS ; L = 22 INCHES
B. # 5 BARS ; L = 27 INCHES
C. # 6 BARS ; L = 32 INCHES

- NOTES:
1. THE MAXIMUM WIDTH BETWEEN LONGITUDINAL JOINTS SHALL NOT EXCEED 15'-0".
 2. ALL EARTHEN AREAS ARE TO BE HYDROMULCHED UNLESS SHOWN OTHERWISE ON DRAWINGS.
 3. CONTRACTOR MAY SAW CUT IN LIEU OF DEFORMED METAL STRIP.
 4. USE STRIP OF SOD GRASS TO PREVENT EROSION UNTIL STAND OF GRASS IS ESTABLISHED.
 5. AN EQUAL OR LARGER AREA OF WELDED REINFORCEMENT BAR CONFORMING TO ASTM A497, MAY BE SUBSTITUTED FOR REBARS LISTED IN TABLE 1.
 6. IF AVAILABLE ROW IS NOT SUFFICIENT TO ACCOMMODATE SIDEWALK WIDTH (SW) ACCORDING TO IDM REQUIREMENTS, ENGINEER SHALL OBTAIN A VARIANCE FROM THE CITY ENGINEER.
 7. REFER TO CONTRACT DRAWINGS FOR PAVEMENT WIDTH (PW) AND PAVEMENT THICKNESS (D), MEDIAN (M), PEDESTRIAN REALM (PR), SIDEWALK (SW), FRONTAGE BUFFER (FB), AND SAFETY BUFFER (SB) WIDTHS.

| | |
|---------------------------------------------------------|-------------------------------------------|
| APPROVED BY: CITY ENGINEER | APPROVED BY: CITY TRAFFIC ENGINEER |
| APPROVED BY: DIRECTOR OF HOUSTON PUBLIC WORKS | |
| EFF DATE: NOV-27-2023 | DWG NO: 02751-01 |
| CITY OF HOUSTON HOUSTON PUBLIC WORKS STANDARD | |
| CONCRETE PAVEMENT DETAILS | |
| FOR CITY OF HOUSTON USE ONLY | |
| DRAWING SCALE | |
| NOT TO SCALE | |